



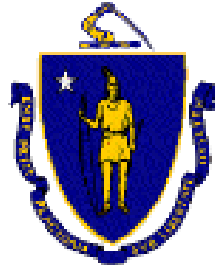
MASSACHUSETTS DEPARTMENT OF  
CONSERVATION AND RECREATION

# **CLIPPERSHIP CONNECTOR**

## **MULTI-USE PATH PROJECT**

**DCR Public Meeting**

**Thursday, June 8, 2017 – 7:00pm to 8:30pm**  
**Andrews Middle School Cafetorium**



# Commonwealth of Massachusetts

Governor

**Charles D. Baker**

Lieutenant Governor

**Karyn E. Polito**

Energy and Environmental Secretary

**Matthew A. Beaton**

Department of Conservation and Recreation Commissioner

**Leo P. Roy**



## **DCR Mission Statement**

*To protect, promote and enhance our  
common wealth of natural, cultural  
and recreational resources  
for the well-being of all.*



# Purpose of Tonight's Meeting

- To introduce the DCR Design Team
- To present the existing conditions of the project location
- To obtain feedback on concept design alternatives for the Clippership Connector

# Project Partners

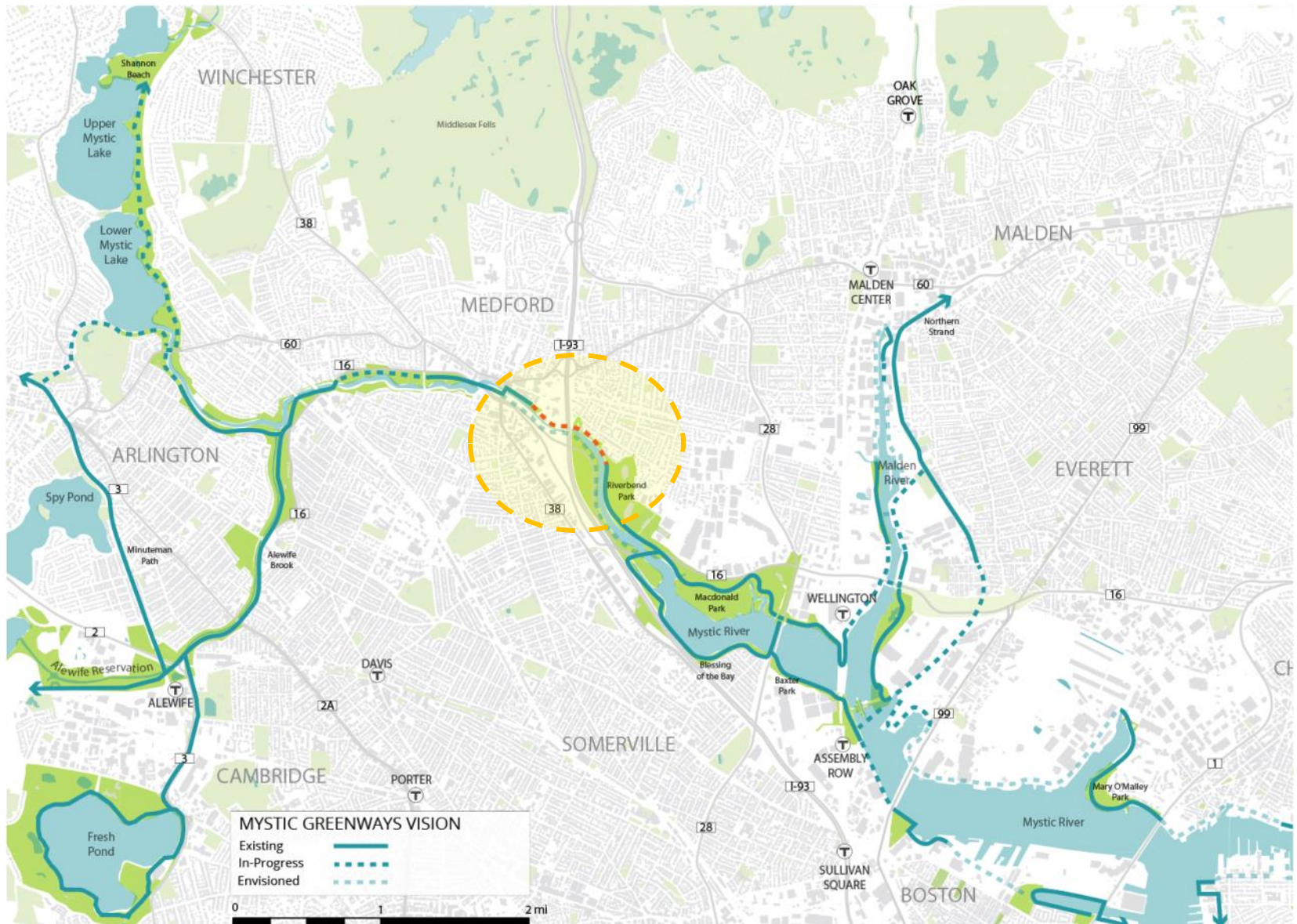


Medford Bicycle  
Advisory Commission



WalkMedford

## REGIONAL GREENWAYS INITIATIVE: Connecting Charles River/Alewife to Mystic River





# Design Team

- Crosby | Schlessinger | Smallridge  
Landscape Architecture, Community  
Engagement
- BSC Group  
Electrical & Civil Engineering
- Hardesty & Hanover  
Structural Engineering
- Epsilon Associates, Inc.  
Environmental Permitting



# Design Goals & Guiding Principles

- The Connector is part of the ongoing implementation of the Mystic River Master Plan
- The Connector shall be direct and shall maximize opportunities for users to view and be close to the River
- The Connector shall be designed primarily as a community route while recognizing the regional network
- The Connector shall be accessible and inviting to people of all ages and abilities, including students and seniors
- The Connector shall be designed to protect the privacy of and address the security concerns of abutters
- The Connector shall improve the habitat and ecology of the river bank
- Other?



# Design Goals



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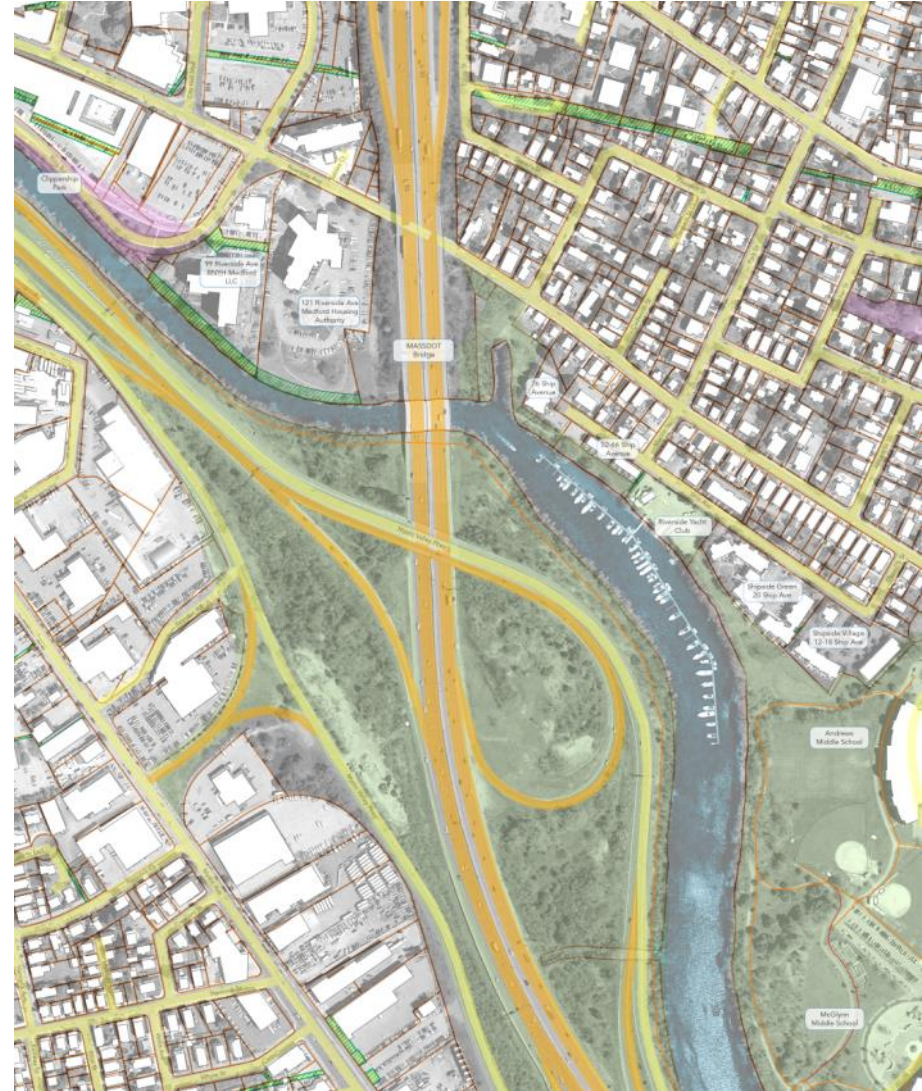
# Design Goals



- The Connector shall improve the habitat and ecology of the river bank



# Existing Conditions





# Existing Conditions

## 99 Riverside Ave





# Existing Conditions

## 121 Riverside Ave



# Existing Conditions

## MassDOT 1-93 Bridge





# Existing Conditions

## Drainage Outlets at MassDot Bridge





# Existing Conditions

## DCR Parcel at Outfall





# Existing Conditions

## DCR Parcel behind 76 Ship Ave





# Existing Conditions

## DCR Parcel behind 52-66 Ship Ave – River Side



# Existing Conditions

## 52-66 Ship Ave – Street Side





# Existing Conditions

## DCR Parcel at Riverside Yacht Club



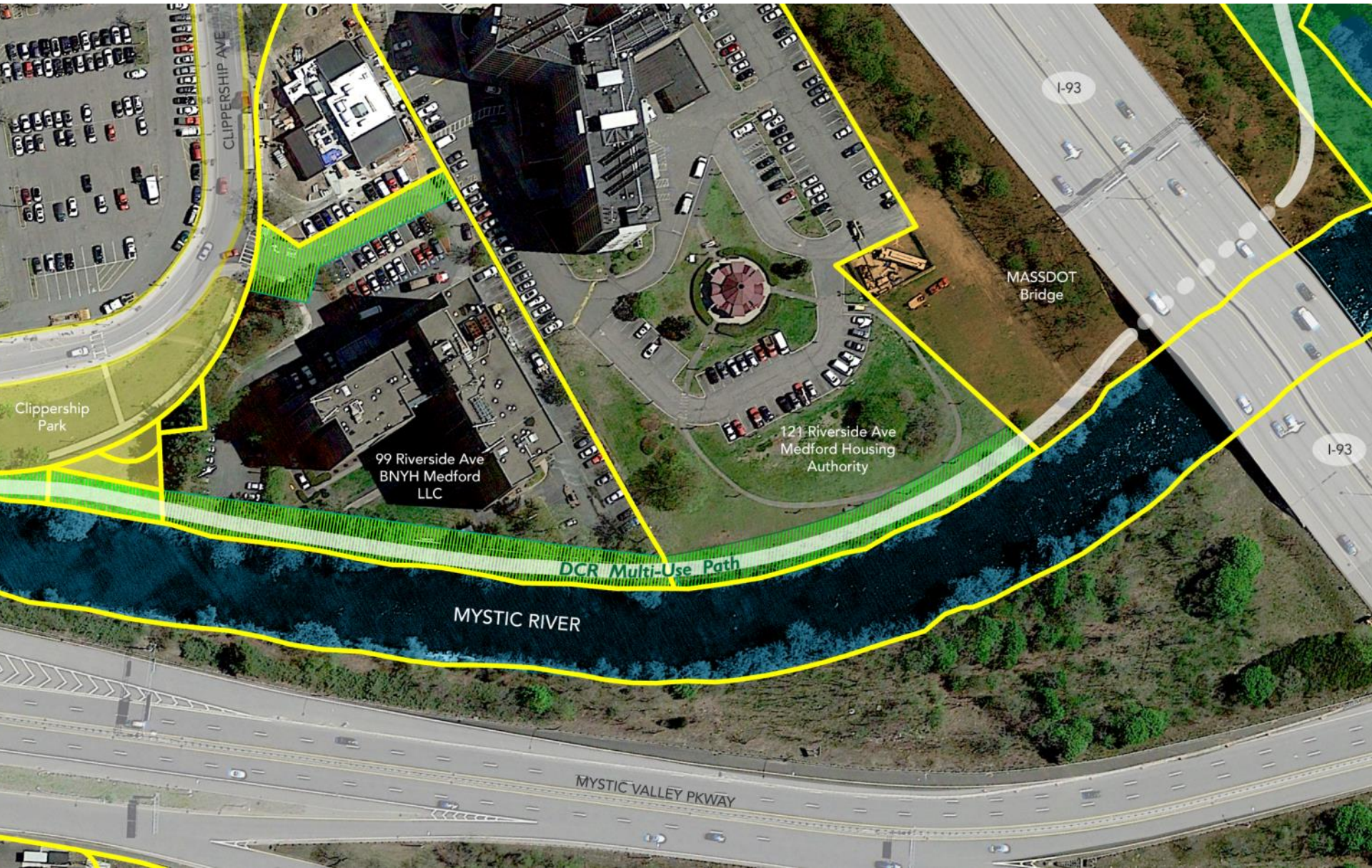
# Concept Design Alternatives

- The Connector is part of the ongoing implementation of the Mystic River Master Plan
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- Other?





# West Section





# East Section- Alt 1





# East Section- Alt 1

## Pros

- Path alignment provides the greatest continuous connection to the Mystic River
- Path alignment is entirely within DCR property
- Path alignment is completely off-road and avoids all on-street vehicle conflicts

## Cons

- Path is in close proximity to abutter backyards
- Path must make 90 degree turns at Riverside Yacht Club
- Path alignment results in potential loss of parking spots at the Riverside Yacht Club



# East Section- Alt 2





## Pros

- Path alignment avoids being in close proximity to abutter backyards
- Path alignment expands distance between 90 degree turns
- Path alignment provides more direct access from residents near Ship Avenue

## Cons

- Path alignment is not entirely on DCR property and will require easement negotiations
- Path alignment will result in loss of public on-street parking spaces
- Path alignment has more user/vehicle conflicts and may deter potential user groups

# Next Steps

- Summer/Early Fall 2017: DCR meetings with abutters and stakeholders
- Fall 2017: Preliminary Design Public Meeting
- Late Fall 2017: Permitting
- Winter 2017/2018: Final Design

# Questions for Discussion

- Are there any other goals or design principles that should be added to the list?
- How would you use the Connector when built?
- What other groups of people might use the Connector?
- Where should connections to the new trail from the neighborhood be located?
- Are there places that should be considered for seating or overlooks?
- What is most important to you to consider in the design?
- What are your concerns about this project?



## Additional Information

### **For more information:**

<http://www.mass.gov/eea/agencies/dcr/public-outreach/public-meetings/>

### **If you have comments or suggestions on this project:**

*Submit online:*

<http://www.mass.gov/eea/agencies/dcr/public-outreach/submit-public-comments/>

*Write:* Department of Conservation and Recreation  
Office of Public Outreach  
251 Causeway Street, Suite 600  
Boston, MA 02114

*Deadline: Tuesday, July 11, 2017*

*Note: Public comments submitted to DCR may be posted on the DCR website in their entirety.*

**If you wish to subscribe to a DCR general information or project-related listserv:**

Contact DCR's Office of Community Relations at 617-626-4973 or [Mass.Parks@state.ma.us](mailto:Mass.Parks@state.ma.us).